Misson Design Guide

Produced
May 2016 for
the Misson
Neighbourhood
Plan

urban forward ltd

Contents

urban forward ltd

Part 1:
Principles

Purpose of this document

This Guide has been produced to support the policies in the Misson Neighbourhood Development Plan. The character and identity of a place, the way it can function, its economic and social qualities, and the amount of energy used by its residents are all influenced by design. It is intended to help developers and designers maximise the opportunities to contribute positively to making Misson a better place by ensuring that any new development contributes positively to the village.

The main aim of this document is to help future design teams produce new development that both embodies the core principles of placemaking and which reflects the specific design qualities of Misson, helping to preserve and enhance its character

There is currently a proposal in development to introduce Conservation Area status to some or all of Misson, lead by Bassetlaw District Council. Should this be pursued, the extra design criteria will need to be introduced. This Guide, in using a heavily place-based approach to design guidance, can been seen as a first step to ensuring new development does not damage the existing character of Misson.

This Guide is set out in three sections:

Section 1 sets out general principles for any design to follow, including how streets are arranged, how landscape should be integrated, and how buildings should work to create safe spaces internally and externally. This section also features excerpts from *Successful Places: a Guide to Sustainable Housing Layout and Design* (NEDDC SPD 2013). Designers are expected to work with this SPD along side this Guide when developing proposals.

Section 2 takes each street and space within Misson in turn, explaining the features that give it its character and drawing out lessons that should help future development be sensitive and in character with the existing settlement.

Section 3 provides useful further reading, including a glossary of terms and an overview of helpful documents.

To ensure development is of the highest quality, designers are encouraged to use this document in conjunction with Building for Life 12, the Government-endorsed standard for well designed residential neighbourhoods, and Manual for Streets, the Government guidance on highways design (see Appendix 1).
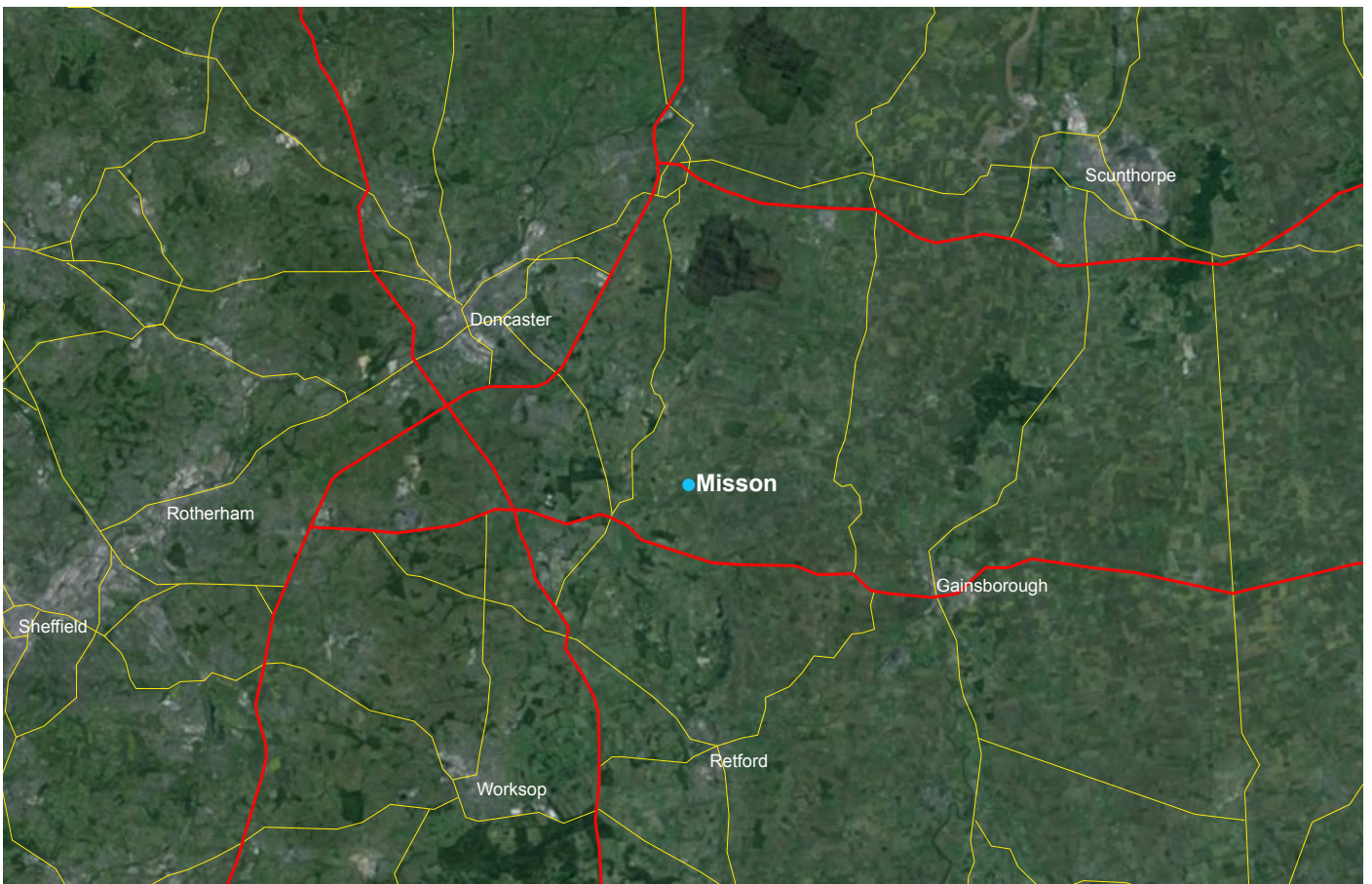
Above: Misson in its wider context

The importance of good design

Good design is about more than just aesthetics; well-designed places let people have better lives by making places safe, easy to move through, economically and socially vibrant, and robust against climate change. Design has a role to play in all aspects of how a place functions; it influences the movement economy, the level of walking and cycling, the way in which people can meet and socialise, where people can take recreation and leisure activities and the levels of crime within any given area.

How a neighbourhood is connected to its surroundings is an extremely important factor when determining the likelihood of residents from that area walking and cycling.

Research has shown that poorly connected neighbourhoods have far lower walking and cycling trips than those that integrate well with local shops and other facilities, which in turn leads to more traffic, poorer air quality and higher health problems for people living there.

Designs that incorporate natural and existing site features into their layout help retain character and identity as well as helping to maintain ecology and biodiversity. If managed well and 'designed in', floor prevention measures can be of real amenity value to local people and a habitat for wildlife.

Buildings that do not properly manage public and private space offer poorer quality living environments for residents, and cannot provide the levels of overlooking needed to make public spaces safe to use. Public spaces which are not overlooked are often sites of antisocial behaviour and are not usable for play and leisure.

However, well designed open space increases people's levels of exercise and give people spaces to meet and socialise. Embedding character into new development helps an area as a whole be more recognisable, and helps to maintain links to a place's history.

Getting things wrong is extremely costly, as many design mistakes last a very long time, having impacts that extend for decades and which can be expensive to rectify. That is why it is critical to embed good design from the outset and to make sure that all new development follows urban design best practice.

"*Good quality design is an integral part of sustainable development. The National Planning Policy Framework recognises that design quality matters and that planning should drive up standards across all forms of development. As a core planning principle, plan-makers and decision takers should always seek to secure high quality design.*"

Source: *PPG: Design* (2015)

Working with the site

Working with what you have on site enables new developments to make the most of their setting, embedding existing landscape and other natural features into the design in a way that helps maintain links to the history of the area whilst retaining the character of the site.

New development in Misson should seek to work with the landscape, retaining important trees and other ecological features and using the topography to influence the alignment of streets.

Existing trees and vegetation should, where practical, be retained in such a way as to add visual amenity and ecological value to the development. Existing trees and hedges can give new development a mature look and feel, and this adds value. However, difficult to maintain or manage greenery should be avoided, as this has the potential to cause problems in the future.

Surface water should be managed in a way that enhances the public realm and provides habitat for wildlife. Sustainable Urban Drainage (SUDs) have the potential to add extra character and amenity to developments but must be considered at the outset along with the design of streets and other spaces rather than retrofitted as an add-on.

> "*A system of open and green spaces that respect natural features and are easily accessible can be a valuable local resource and helps create successful places. A high quality landscape, including trees and semi-natural habitats where appropriate, makes an important contribution to the quality of an area.*"
>
> Source: *PPG: Design* (2015)

Sucessful Places says:

"*Proposals should integrate green and blue infrastructure into the development layout wherever possible.*

Developments should create places of character based upon an appreciation of the site and surrounding area, responding positively to its natural and built context.

Developments should be orientated to benefit from passive solar energy.

Developments that form a new long term settlement edge should create a positive relationship with the adjoining countryside, providing an appropriate transition between the built up area and the adjoining landscape.

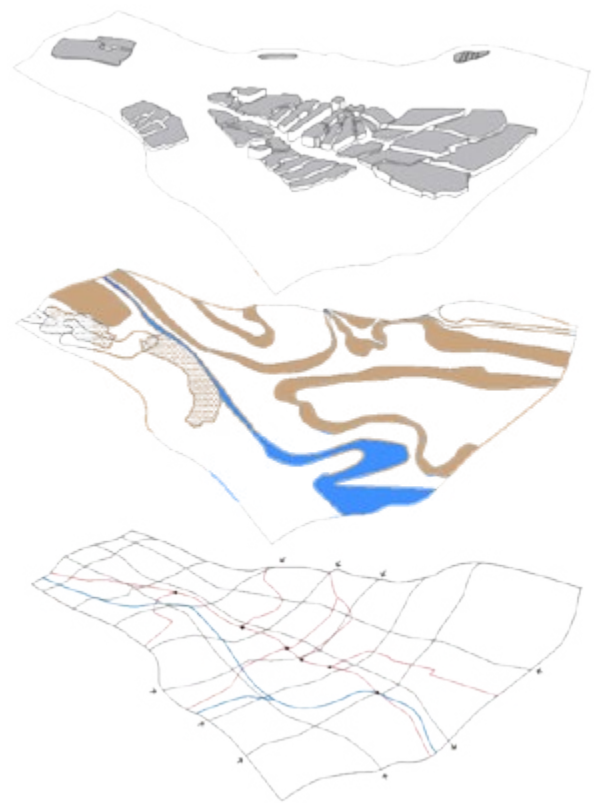*Planting should create and reinforce character, scale, continuity and variety throughout the seasons.*"

Source: *Successful Places: a Guide to Sustainable Housing Layout and Design* (NEDDC SPD 2013).

Right: Landform and watercourses influencing route structure and developable land.

(Source: Sue McGlynn)

Images: Existing trees and water being used to generate place character and identity.

> "*Development proposals should promote accessibility and safe local routes by making places that connect appropriately with each other and are easy to move through. Attractive and well-connected permeable street networks encourage more people to walk and cycle to local destinations.*"
>
> Source: *PPG: Design* (2015)

Connecting to the neighbourhood

Movement is the lifeblood of settlements; places with well-integrated movement systems have been shown to be economically and socially vibrate, safe and energy efficient. New development in Misson should not shy away from making strong links with the local neighbourhood, nor should it seek to create insular and overly private enclaves within existing neighbourhoods.

Instead, new development should make as many links with the surrounding street mesh as possible, but only where those links can be well overlooked, direct and legible. Routes that are poorly overlooked, that run adjacent to private gardens of between back fences, or that are unnecessarily indirect should be avoided.

Streets should be designed in a way that offers more than just a movement corridor for cars; they should be pedestrian and cycle friendly, have space for parking, and should slow traffic through their design rather than through retrofitted calming measures.

Internally, where possible new streets should form a grid, with as many streets offering through movement as possible. Where this is not practical, dead ends should be short and should not be connected by blind alleys. Streets should vary in character, with their role in either local or wider movement evident from their design.

All streets should be simple and uncluttered, with decent lines of sight, low speeds and space for trees. Over-engineered junction radii should be avoided, and all streets should be design to Manual for Streets principles.

Developers should also seek to ensure they take regard for the many footpaths that connect the village to the wider countryside. Connecting to and improving the existing footpath network should be a key consideration for designers.

***Sucessful Places* says:**

"Layouts should provide a linked network of routes and spaces within the development and connect to adjoining areas.

Roads should be safe, inclusive and an integrated component of the design in a way that helps create streets and places not just roads for carrying traffic.

Using a simple palette of complementary materials, the architecture of an area and the activities of its inhabitants should give character to the streets. The choice of hard materials must reflect this intrinsic street character whilst also achieving continuity of movement, flow and, with it, connectivity."

Source: *Successful Places: a Guide to Sustainable Housing Layout and Design* (NEDDC SPD 2013).

Right: A grid of streets enables high permeability and easy movement, where as a nested hierarchy offers very little route choice.

Bottom right: Quieter streets like this mews in Upton can be more pedestrian focused.

Bottom left: Mature trees and good sight lines give this street a high quality feel.

High permeability

Low permeability

> *"Development should seek to promote character in townscape and landscape by responding to and reinforcing locally distinctive patterns of development, local man-made and natural heritage and culture, while not preventing or discouraging appropriate innovation."*
>
> Source: *PPG: Design* (2015)

Making a place

All new development in Misson should seek to promote local character and identity, because through doing so it is possible to protect and enhance what is already there for existing residents, and provide community and social cohesion for those new to the area.

A criticism often levelled at new development is that it 'lacks character', with many new developments looking generic despite the wide range of building types and materials used. Often this is due to overly standardised approaches to streets and spaces, where very little room is given for the types of innovation that allow one place to be different from another. Also, too wide a range of materials and styles can confuse the identity of new development, with the lack of a coherent approach weakening the overall visual quality and diluting the overall character.

To maintain local distinctiveness, new development should be reflective of local aspects such as:

- the local landform and the way development sits upon it
- the local pattern of streets, blocks and the dimension of plots
- development style and vernacular
- built forms, massing, details and materials (including for street furniture and boundaries)

Of particular concern within Misson is parking. Many of the streets suffer from a lack of integrated parking, leading to poorly managed and arranged on-street parking that clogs the street. As a general rule, developers looking to provide new homes in Misson should seek to ensure that there is provision for on-plot parking at a level that suits the needs of the dwellings, likely to be at least two spaces, more for larger units.

Developers should demonstrate how they have embedded local character in their Design and Access Statement.

Source: *Successful Places: a Guide to Sustainable Housing Layout and Design* (NEDDC SPD 2013).

Sucessful Places says:

"A development, depending on its scale and context, should provide variable densities to support areas of character, the viability of local services, facilities and the landscape setting of the area.

On-plot parking and garages should be sited so they do not appear as dominant features in the street scene and be sufficient size to function as a parking space.

Using a simple palette of complementary materials, the All schemes should provide a level of outdoor amenity space that is proportionate to the type of accommodation, appropriate to its location and suitable to meet the occupiers likely requirements.

Boundaries should be appropriate to their location, strengthen distinctiveness and reflect the characteristics of the local context.

Building forms and details should be appropriate to the local context, their position and role within the place hierarchy and make a positive contribution to the character of the place.

The potential for a dwelling to be extended should be a consideration at the design stage providing this would be appropriate to the character of the development and its context.

Building materials and colours should be chosen for their high quality, to complement site context and to strengthen the local distinctiveness of the area

Materials should be durable, robust and maintainable and chosen with regard to their visual qualities and contribution to the character of the area

Each dwelling should have an adequate storage area for refuse and recycling containers, designed and sited so as not to detract from the appearance of the development and to allow bins to be safely and conveniently taken to the collection point."
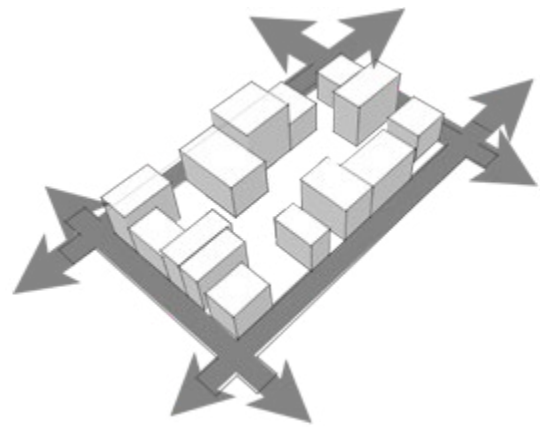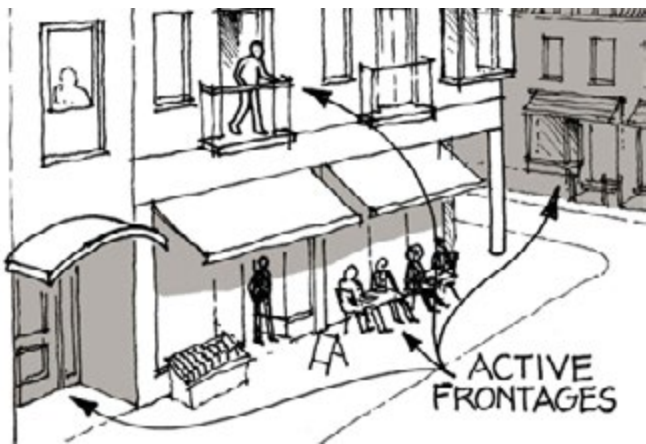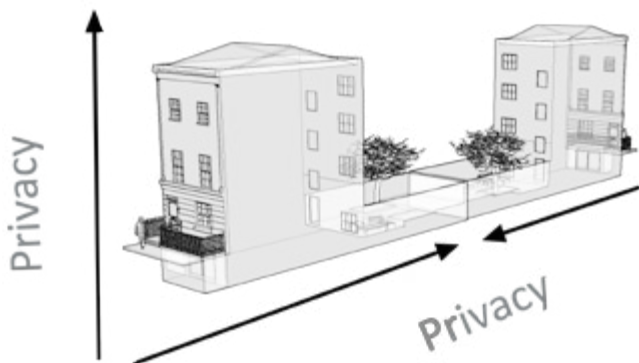
Clockwise top left:

Active building fronts provide overlooking to the street, making it safer.

Buildings arranged in a perimeter block, with public streets and spaces to the front, and private gardens to the rear.

Perimeter blocks set up a 'privacy gradient', enabling active frontages whilst keeping gardens and rooms within a building private.

Good design checklist

Below is a simple checklist to help designers when thinking about how to bring a site forward. It may also be helpful for other stakeholders looking at a development proposal, prompting examination of the design elements that are often left until too late in the design process or overlooked all together.

A more comprehensive checklist can be found in Building for Life 12 (see Appendix 2).

"The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people."

Source: *National Planning Policy Framework* (2012)

Are there existing site features of note?

Can these be integrated into the development to add character and preserve site identity?

How can new routes into and out of the development help link with existing areas and make finding your way around easy?

How should they cross the site?

Where should vehicles come into and out of the development?

Are there any traffic issues to manage? Parking is a big issue in the village; how is this to be addressed?

Where should pedestrians access the site?

Are there any existing rights of way to consider?

Are streets designed to be pedestrian friendly so as to encourage walking?

Are vehicle speeds low and are there places to meet and socialise?

What are the needs of cyclists in the area and how have you accommodated these?

Is there enough parking for bikes?

Are buildings and spaces designed to be safe? Do buildings face the street and are their gardens secure?

Are public spaces well overlooked and do they have a clear use?

How are bins and recycling to be dealt with?

Where are bins stored? Can people put bins away when after waste has been collected?

Part 2:
Design Guide

Streets and spaces

In order to understand and explain the townscape character of Misson, a street-by-street analysis has been undertaken. In all, 12 study areas have been identified. Where possible, these have been drawn so as to include whole streets or spaces and their edges.

Inevitably, there is some overlap between the character of one area and another, and although every effort has been made to define areas in a way that establishes difference, designers and developers should look to the complete analysis rather than focusing on just one street or space when considering how to bring forward new design.

Coronation Avenue

Newington Road

Village Green

Back Lane

West Street

The Bungalows

High Street

Dame Lane

Middle Street

Manor Close

Top Street / Top Road

Station Road
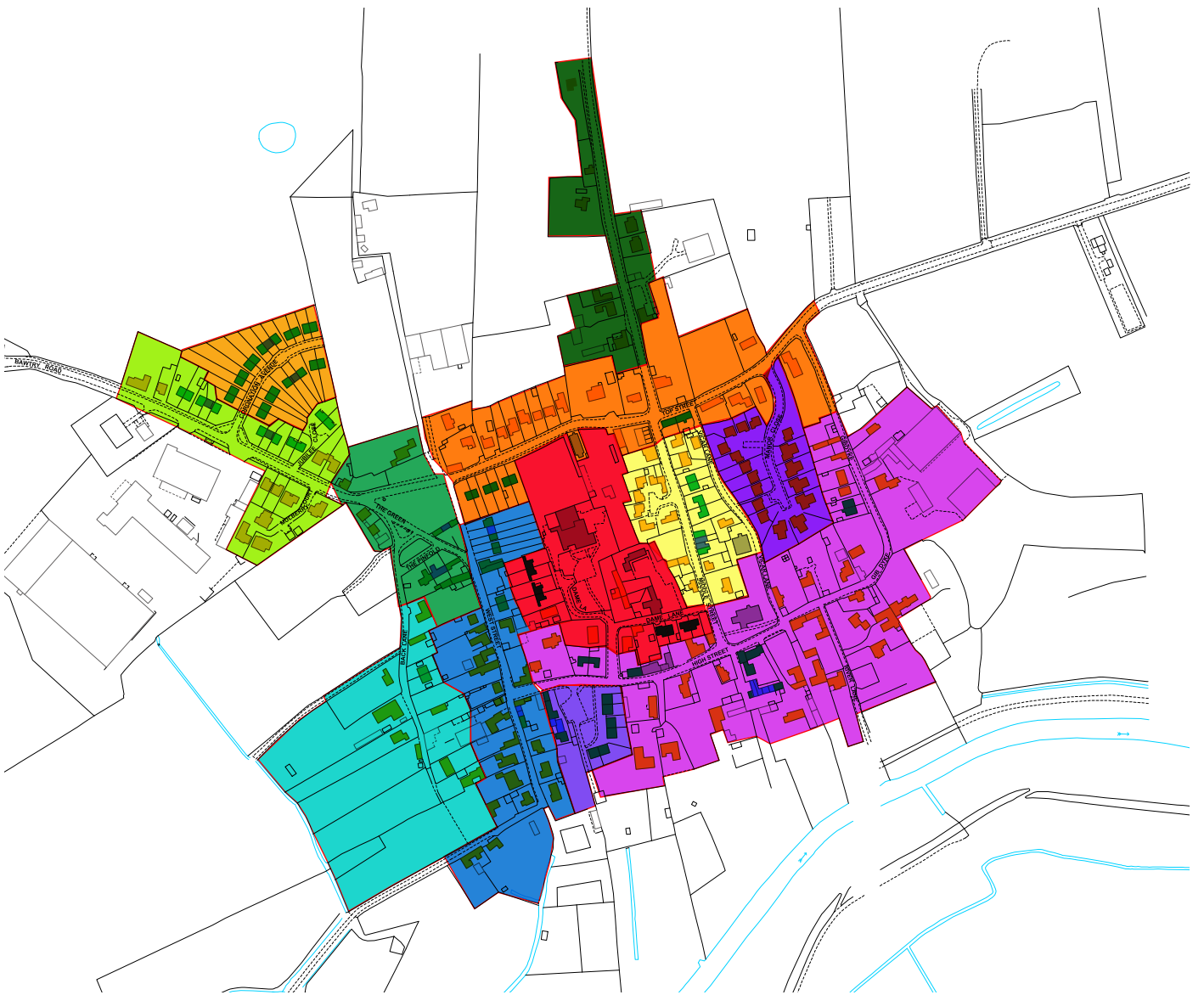
Dwelling types

How buildings relate to their plots and to each other is an important character-forming element for any settlement; the way scale and massive, build-to-plot ratio and building set back are arranged on a street help to establish a street's role in the overall structure of a place and make understanding how it is put together easier for users.

For Misson, a key characteristic is that a high proportion of the dwelling stock is detached. Higher-order streets such as Top Street and the High Street are characterised by detached dwellings, and this helps to reinforce their importance within the village.

Smaller, more dense housing can be found on back lanes and side streets, signalling their more local nature. Sporadic terraced housing can be found, but this is rare in the context of Misson; new development should seek to use only a minimal number of terraced housing.

Detached

Semi-detached

Terraced

Non-domestic
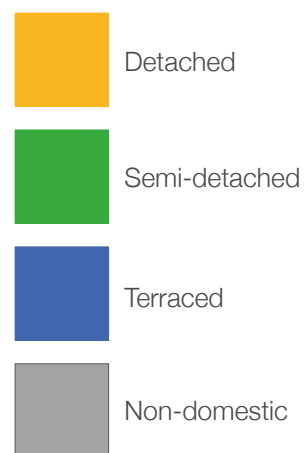
Newington Road

Newington Road is the main approach to the village from the west. It features dwellings similar to those on Coronation Avenue, but here you also find other types and materials. This increases the complexity of the townscape in this street and gives in its own character. The transition from the rural edge to the more urban core occurs through this space.

Plot, building and street dimensions typically found in the area.
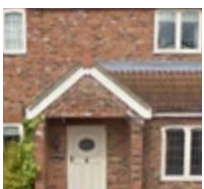
Materials

Red and brown pantile roofs, some concrete roof tiles.

Occasional render in white with contrasting weatherboarding.

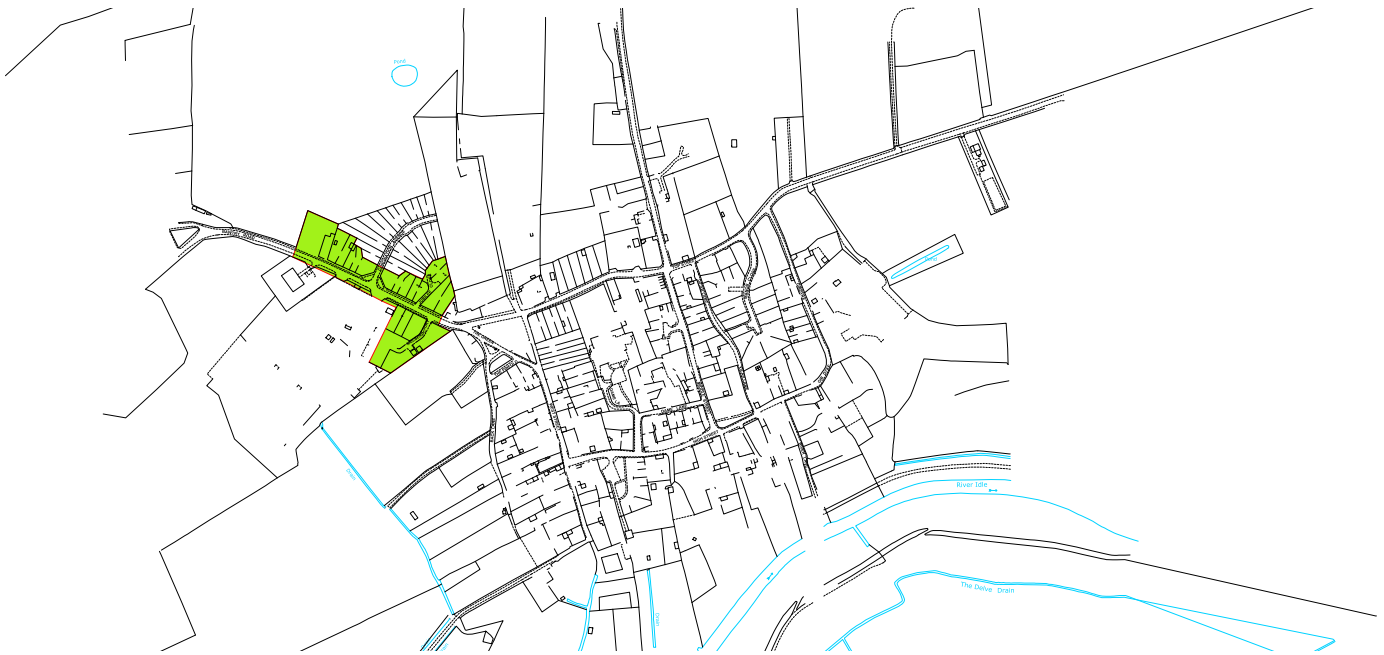Multi-grain brick in reds and browns.

Details

Brick coursing details for some buildings.

On-plot parking to the side of dwellings.

Hedges and verging to parts of the street. Occasional street trees.

Street role and form.	Global integration, with connecting power between the village and other places and spaces beyond the extend of the settlement. Sight lines are very long at around 280m.
Plot range	Varied, with some wider plots on corners, but more uniform between these. Generally within the range of 8-12m but with plots to the edge of the village up to 28m.
Building to plot ratio	High, typically 70-80%, although some houses occupy 100% of the plot width.
Building set back	Varied, and increasing as you head into the village. The range is 5-15m, but with most within the range of 8-12m.
Roof scape	Mainly open gables with the ridge running parallel to the street. Front projecting gables perpendicular to street on some dwellings. Roofing materials are concrete tiles or red pantiles. Most properties have chimneys.
Parking	All on-plot, mainly placed side-of-plot. Some on-plot to dwelling fronts. Although there is scope for on-street parking, the traffic volumes here make it undesirable.
Landscape and boundaries	Front boundaries consist of either low walls, post fences, or clipped mono-culture hedging. Small garden trees add extra greenery to the street, as do trees on public corner spaces and within the mill site.
Scale and massing	Mainly two storey, with some bungalows. Some porches, although no uniform approach to scale and massing. All buildings are of a domestic scale.

Lessons

The scale of the plots and way buildings sit on them help to establish this street as an important movement corridor for the whole village. Buildings sit set back from a clearly defined boundary, in generous plots with ample on-plot space for parking. The larger gardens allow for bigger tree specimens, and these help define the street edge and reduce the appearance of built form. New development should seek to replicate the overall scale and approach when looking to provide new streets with a high movement function.

Coronation Avenue

Coronation Avenue is located to the north west of the village, and is a no-through road of mainly semi-detached houses. The street is very uniform, with similar house types set in a standardised plot. It is typical of inter-war home building and is a good example of this era of development.

Plot, building and street dimensions typically found in the area.
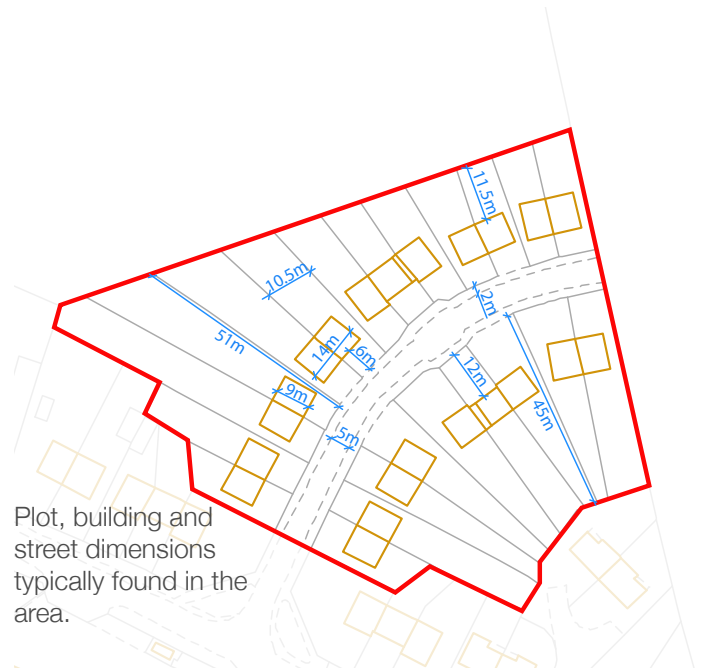
Materials

Red-brown bricks, stretcher bond.

Concrete roof tiles, in grey-brown.

Windows in white uPCV.

Details

Monoculture boundary hedging, formally clipped and around 1.2m high.

Porches to front, with entrances set back within building line.

White soffits, barge boards and rain water goods.

Shared, centrally placed chimney stacks with four pots.

| Street role and form.	Local access only, with no connecting role for wider movement. Standard dimensions, with pavement to either side. Sight lines of approximately 70m, with 'open' angular deflection.
Plot range	Narrow, typically similar in width although varied in length to allow for the shape of the site. 8-10m plot width is used, with lengths from 30-50m.
Building to plot ratio	High, typically 70-80%, although some houses occupy 100% of the plot width.
Building set back	Relatively uniform at around 6m, but with a few properties on the inside of corners setting back at 12m.
Roof scape	Mainly open gables with the ridge running parallel to the street. Front projecting gables perpendicular to street some dwellings. Roofing materials are concrete tiles exclusively. Shared stacks for chimneys placed centrally, often with four pots.
Parking	Mainly on-plot to the front of dwellings, with some side-of-plot parking for plots on the inside splay of corners. Incidental on-street parking is also available.
Landscape and boundaries	Front boundaries consist of clipped mono-culture hedging, with some fencing and low walling occurring. Small garden trees add extra greenery to the street.
Scale and massing	Exclusively two storey, with single storey porch structures to the properties. Eaves height approximately 6m, 9m to ridge.

Lessons

The uniformity of design here, with a simple range of materials, regular arrangement of plots and buildings, and a strong yet simple boundary treatment could be used on future development to give a cohesive character without attempting to replicate more complex parts of the village. Parking could be better integrated, and more space on plot should be given over to parking. Where hedges have been removed to allow for more parking, the quality of the street is reduced. Protection for boundary hedges may be needed for new development.

Village Green

The village green is a memorable space within Misson. It is a triangular green bounded by lanes and addressed by buildings to its edge. Large trees line the edge of the space and the space to the north, which work together across the street to provide the feeling of openness. The space helps to provide a break in the built form of the main village streets and adds a great deal of character to the village core.

Plot, building and street dimensions typically found in the area.
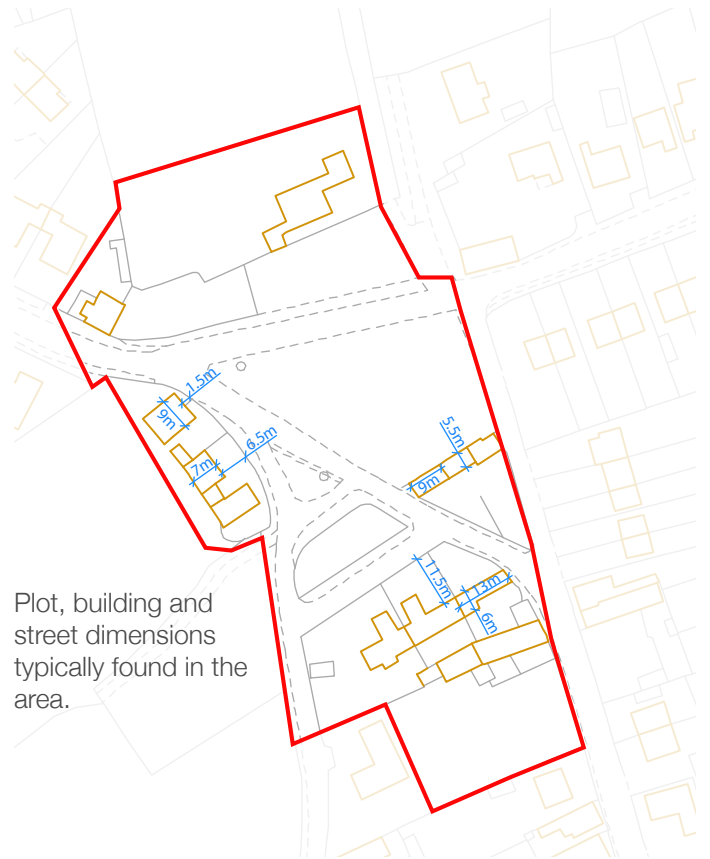
Materials

Render in whites and creams, with both rustic and smooth finishes.
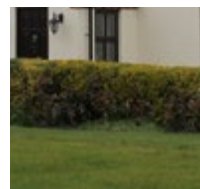

Most buildings have pantile roofs in reds and greys.

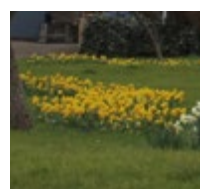
Multi-grain brick to some buildings.

Details

Monoculture boundary hedging, formally clipped and around 1m high.
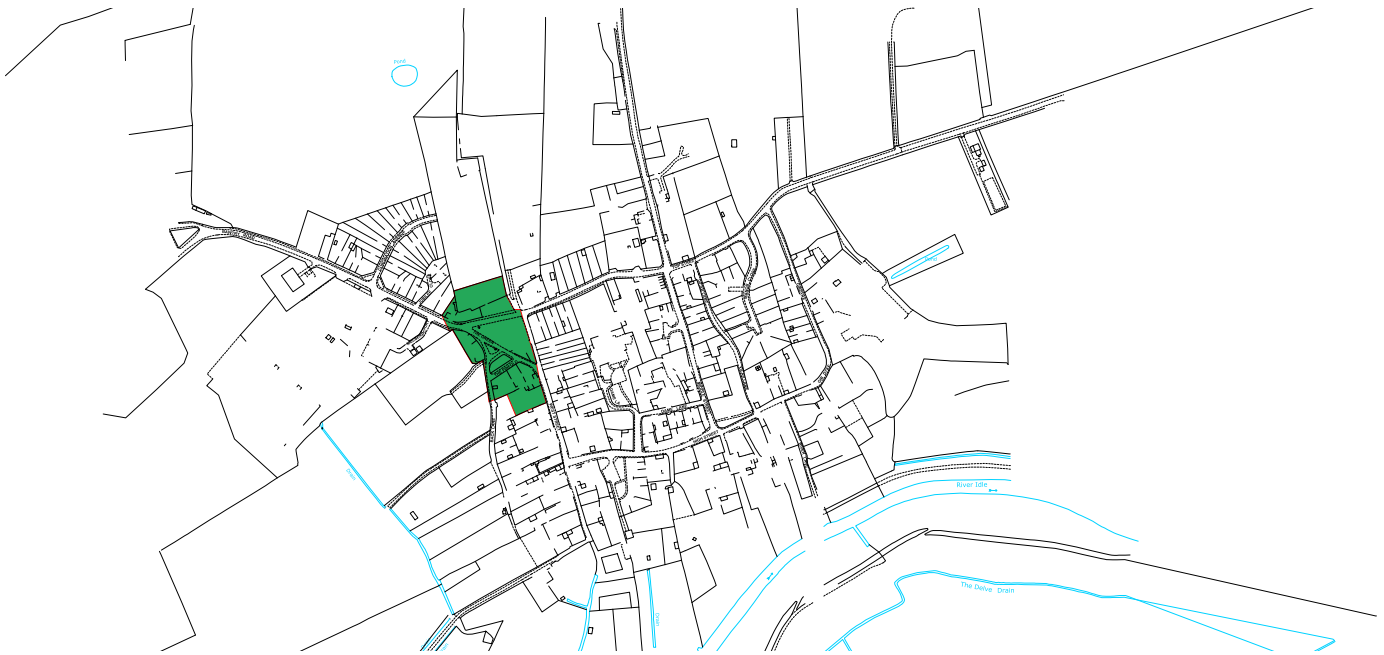

Occasional porches to front, with matching roofing materials.

Trees placed to the street-side edge of the space, providing enclosure.

Some flower planting within mown lawn space.

Street role and form.	Triangular green bounded by lanes. Provides the function of village green public open space.
Plot range	Vary varied, dependant on which edge of the green you are on. Larger plots on higher order streets, smaller ones on lanes to the back edge of the space.
Building to plot ratio	High, including some 100% (terraced) dwellings, although the northern edge is very low at around 20%. Typically 70-80%.
Building set back	Low, with most buildings set at or near the plot line to the edge of the space, giving a feeling of enclosure.
Roof scape	Mainly open gables with the ridge running parallel to the street. Some hipped roofs and hip and valley roofs. All dwelling have chimneys.
Parking	Mainly on-plot to the front or side of dwellings. Some garages. Limited scope for on-street parking.
Landscape and boundaries	Front boundaries consist of clipped mono-culture hedging, high and low walls in brick and render, some railings.
Scale and massing	Two storey, with a range of smaller out buildings on some plots. Eaves height approximately 6m, 9m to ridge, but scale feels smaller due to the width of the space.

Lessons

The village green provides a useful precedent for any developer looking to provide new open space in their development. It is bounded by interconnected streets and lanes that enable buildings to front on to the space and provide overlooking. The trees are placed to enable the street to feel enclosed. Seating is provided which makes the space usable for socialising.

Plot, building and
street dimensions
typically found in the
area.

Back Lane

Back Lane is a narrow route with a rural feel,
characterised by high hedges and walls, with large
properties set in generous plots. It is enclosed by
the boundaries, and the street has no footpath. The
buildings along its edge are often hidden behind the
boundaries, offering limited overlooking to the street.

Materials

Cream and off-white render with
coloured window frames.

Clay pantiles in reds, with white
soffits and barge boarding.

Multi-grain brick, with reds and
browns. No buff materials present.

Multi-grain brick walls to most
boundaries.

Details

English garden bond with brick tops
for boundaries.

Some gates to properties, enclosing
plots from the street.

Fencing atop low walls adds some
visual permeability.

Parking on plot within the grounds.

| Street role and form.	Local access only, with no connecting role for wider movement. Rural lane look and feel, with narrow street widths and no pavement.
Plot range	Reasonably narrow, although splits across the street, with deep plots to the west and shallower, more compact plots to the east, possibly due to subdivision on this side of the street.
Building to plot ratio	Varied, but generally low. Some instances of around 70% of plot, but as low as 30% for some plots.
Building set back	Very varied, with some properties set at the back edge of the lane and others set back up to 30m.
Roof scape	Mainly open gables with the ridge running parallel to the building face. The roofscape appears more varied than this due to how buildings address the plot.
Parking	Mainly on-plot to the side of dwellings, within the grounds. Garages occur for some properties.
Landscape and boundaries	High walls (approx 1.8m) or fences, often with
Scale and massing	Two storey, with single storey porch structures some of the to the properties. Eaves height approximately 6m, 9m to ridge, although may be taller for some properties.

Lessons

Back Lane is characterised by a combination of high levels of enclosure to the street, provided by the tall walls and fencing, and fragmented buildings, with dwellings sitting in a loose and irregular pattern. The combination of a formality to the street edge with less regularity for buildings and plots makes this street a interesting precedent that may make for good inspiration for prestigious but quiet addresses in new development. However, new development should seek to better address the street so that it is overlooked, maybe through reducing the height of boundary walls and introducing some hedges so that homes can interact with the street.
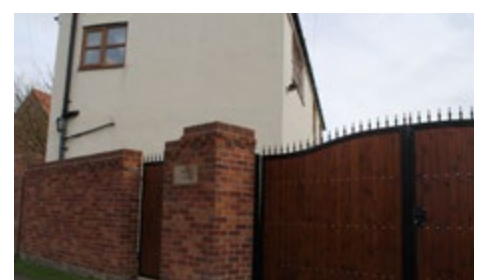
West Street

West Street is a minor integrating route for the whole village, allowing for connections out to the west through a narrow rural lane and for interconnections within the village. It is a relatively formal street, with generally larger properties arranged to provide a relatively consistent edge to the street. Low walls and some trees within private gardens add enclosure to the street, and a varied pallet of materials makes this an interesting area of townscape.
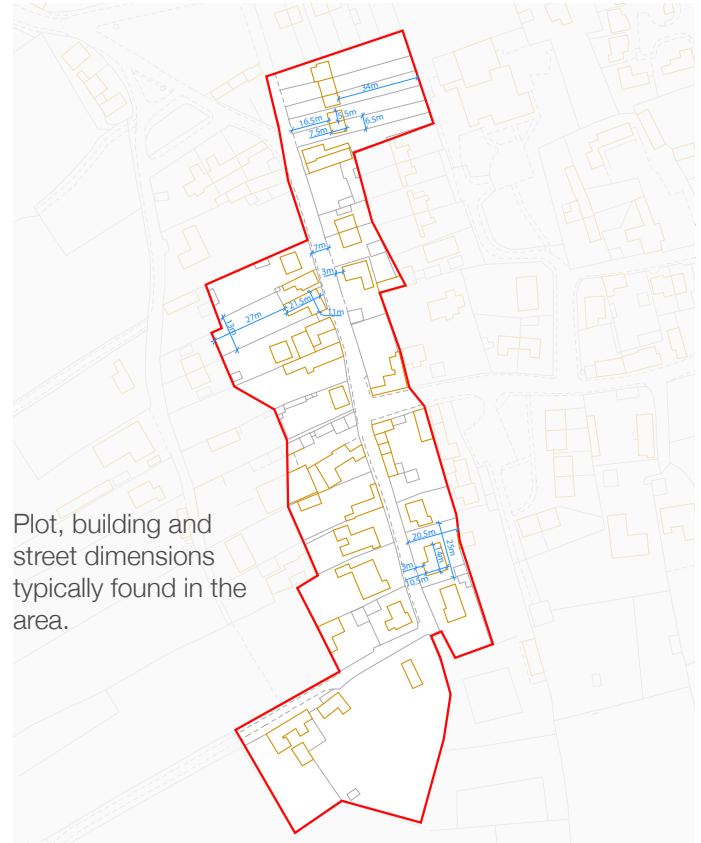
Plot, building and street dimensions typically found in the area.

Materials

Red-brown bricks, stretcher bond.

A mix of pantiles and concrete roof tiles.

Black railings and white windows and detailing.

Red brick walls with garden wall bond, hard up to the back of the footpath.

Details

Green front gardens with small trees add to the street scene.

Garages and parking mainly on plot.

Greenery behind walls adds to the street scene.

Low walls with hedges and greenery behind.

| Street role and form	Global integrator, offering access to settlements beyond the village. Narrower and more minor that the other integrators in the village. Foot path to one side, with the feel of a less urban street. Sight lines long at approximately 220m.
Plot range	Vary varied, with plots ranging from 7-40m, although most are within the 15-25m range. Some plots site 'wide' to the street, with rear gardens running along the edge. Others
Building to plot ratio	Reasonably varied, with some terraced property in with very fragmented arrangements.
Building set back	Varied, with some buildings placed hard up against the back edge of the footpath and others set back with front gardens. Setback does not exceed
Roof scape	Vary varied, with some hipped roofs, some front-facing gables and some gables arranged parallel to the street. Where front-facing gables occur, these give the street a feeling of extra enclosure.
Parking	Mainly on-plot to the side of dwellings, with some garages set within the plot. Incidental on-street parking is also available, although the street is narrow so this must be staggered.
Landscape and boundaries	Mainly low walls in brick, with either hedges or railings above. In some instances, the boundaries are made up of the sides of properties.
Scale and massing	Two storey, although due to the arrangement and alignment of roofs, can feel more. Eaves height approximately 6m, 9m to ridge.

Lessons

West Street is a good precedent for adding connectivity to new development without creating streets that seem overly engineered or that are unpleasantly for pedestrians. The level of enclosure, via either the boundary walls or through properties that sit at the back edge of the footpath, is reasonably tight and gives this street a village feel, helping to make it inviting and attractive. Although varied at the plot level, the continuity of the boundaries and buildings helps to give this street a distinctive character.

8.5m

8.5m

9m

Plot, building and street dimensions typically found in the area.

The Bungalows

The small collection of adjacent to the High Street are not typical of Misson, instead being more typical of their age and type rather than being especially 'in character' with their location. They sit around a small green with trees and offer a housing choice for people wanting to down-size etc.
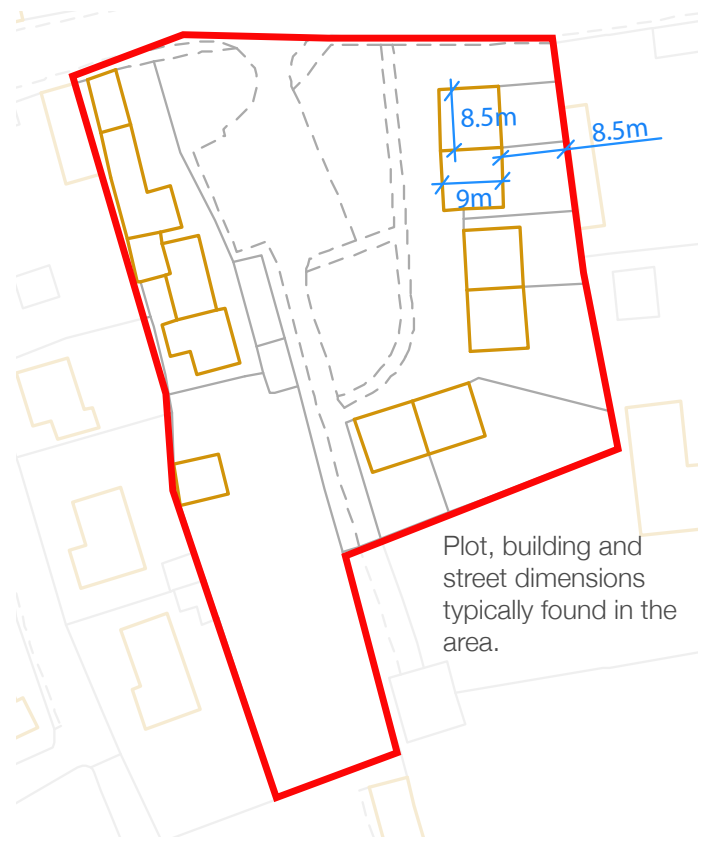
Materials

Concrete roof tiles in grey-brown.

Brown brick with render panels.
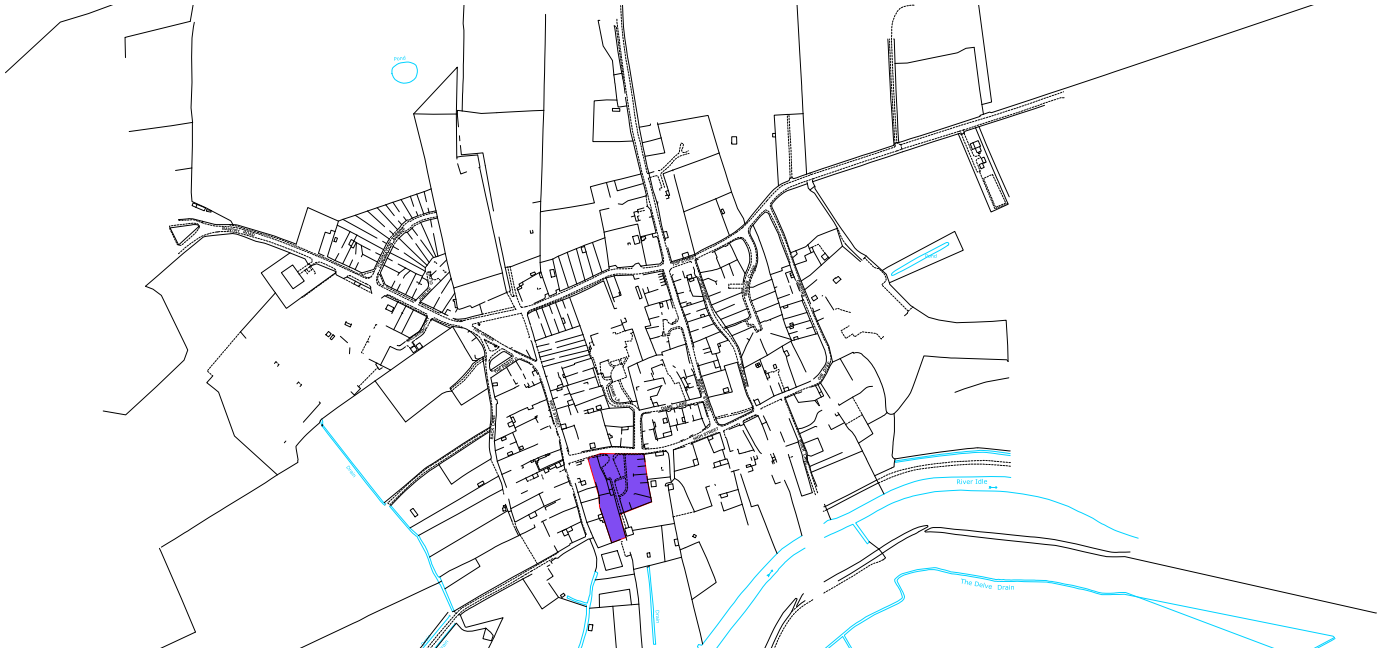
Details

Mown lawn with small trees.

Green space acts as a small village green.

Parking courtyard overlooked by surrounding properties.

Street role and form.	Local access only, with no connecting role for wider movement. Access is via a small drive and parking square.
Plot range	Narrow, with a width of 8-9m and a regular garden length.
Building to plot ratio	High, typically 100% of plot.
Building set back	Uniform at around 5.5m from the back edge of the footpath.
Roof scape	Exclusively open gables with the ridge running parallel to the street. Roofing materials are concrete tiles.
Parking	Provided in a parking square to the front of the dwellings.
Landscape and boundaries	Mown grass, no front boundaries (open frontage). Small trees planted in the communal open space in the centre of the site.
Scale and massing	Single storey, semi-detached.

Lessons

The bungalows offer a small enclave of development that offers a useful injection of different house types to cater for those either downsizing or who want a more manageable plot. Developers could replicate the intimacy of this space, with a communal garden allowing for residents to socialise. The lack of a clearly defined boundary, the overall lack of landscaping and the poorly integrated parking should not be replicated.

High Street / Church Street / Gibdyke

High Street, Church Street and Gibdyke are among the most varied streets in Misson, and are home to a range of townscapes, which together make these streets memorable and character-forming places. Although separate streets, they flow into one another and share enough similarities to enable them to be considered together in this analysis. The varied enclosure, form and building arrangements help to make these streets have their own character in the village, but unifying elements such as materials and boundaries help them remain identifiably in character with the wider settlement.
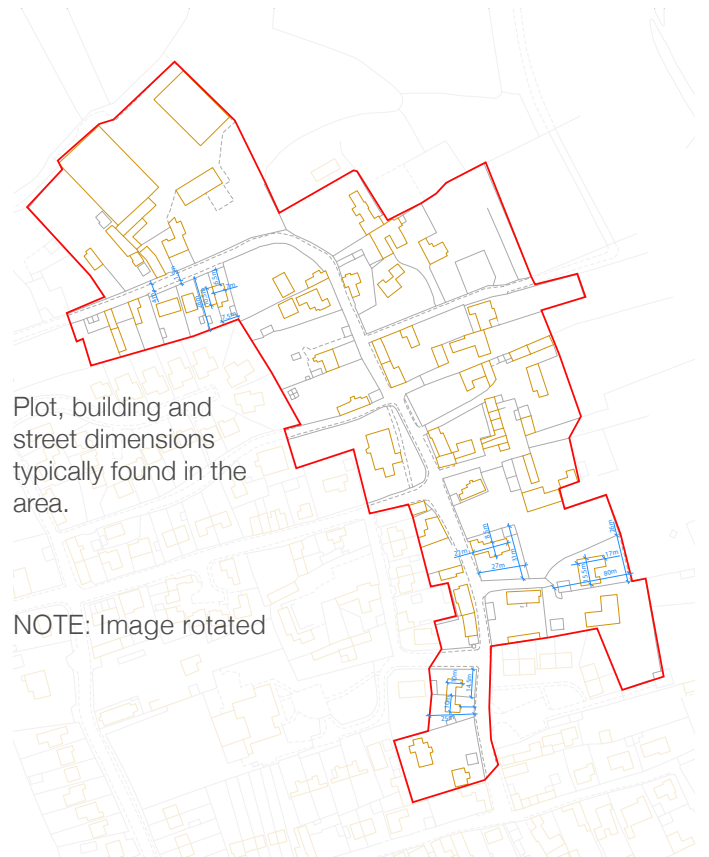

Plot, building and street dimensions typically found in the area.

NOTE: Image rotated

Materials

White render with black plinth.
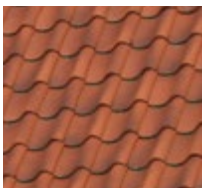
Red pantile roofs.

Red and some multi-grain brick with Flemish bond.

Some renders are rustic in finish.

Details

Brick walls with English garden bond, varying in heights but generally 1.2m approx.
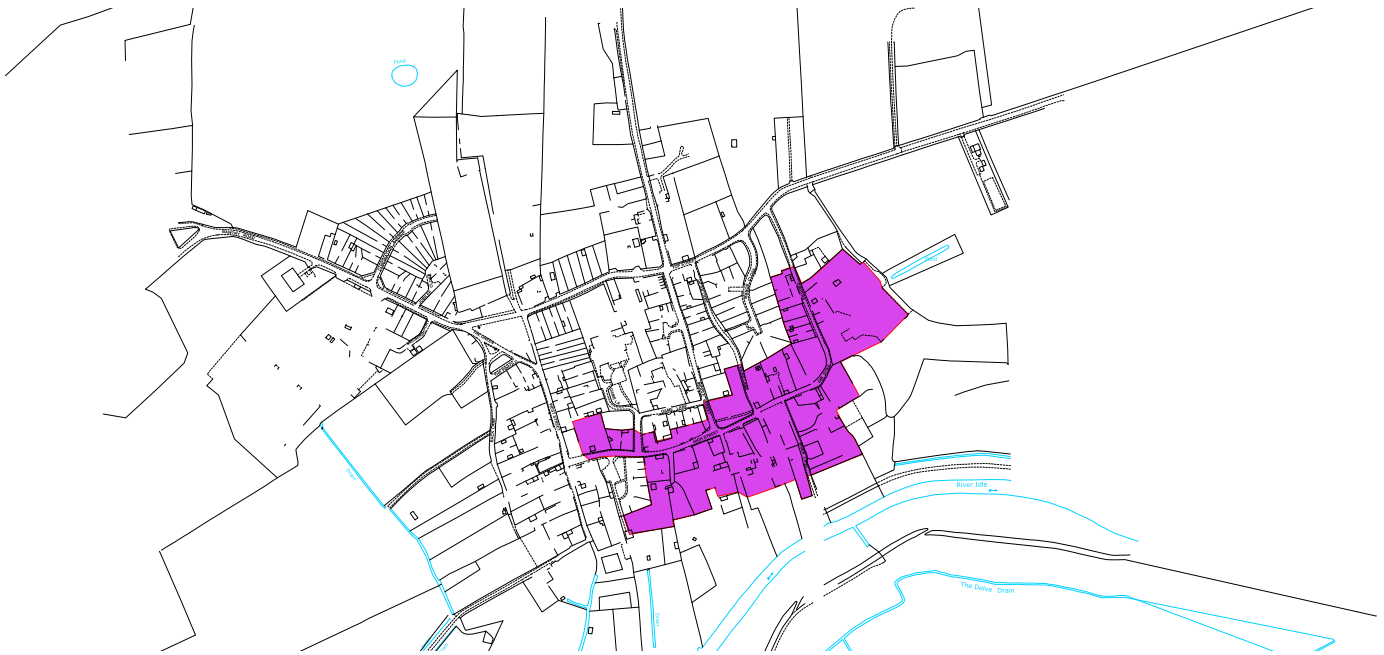

Walls generally topped with a brick on edge coping course.

Some highly detailed feature buildings sit among more simple structures.

Sight lines are shorter here, denoting the more local access role of the street. Enclosure is maintained through walls and boundaries.

|	
Street role and form.	Local grid roads, connecting between streets that integrate the village into a wider movement pattern and more local access streets. Form varies along its length, with some widening and the occasional footpath to both sides.
Plot range	Wide, with plots arranged against the street edge in a variety of ways. The lower range for plot width is still relatively high at around 8m, with some plots presenting their wide edge to the street for widths up to 35-40m.
Building to plot ratio	Vary varied, with some buildings sitting at the front of their plot, occupying 70% of its width to the street, and others sitting appearing to cover as little as 20% due to the plot having its long edge to the street.
Building set back	Varied, but with a maximum set back of around 15m. Some buildings sit hard up against the back edge of the footpath, addressing side lanes with their doors, others sit deeper within their plot addressing the main street.
Roof scape	Varied and complex, with hipped, centre-gully and regular open gables all present. Mono-pitch lean-tos for outbuildings are characteristic. Tall chimneys commonplace.
Parking	Mainly on-plot in a variety of formats, with some garages. Incidental on-street parking is also available.
Landscape and boundaries	High (1.8m) and low (1.2m) brick walls are predominant for boundaries, with hedging and trees behind that green the street. Occasional railings in black.
Scale and massing	Two storey, with single storey outbuildings and garage structures to some properties. Eaves height approximately 6m, 9m to ridge, but some taller chimneys.

Lessons

The rich townscape here uses a great deal of variety at the level of the street, with widening, changes in parking and enclosure etc, and slightly shortened sight lines to create character. The plots and buildings vary in their size and proportions, but are unified through a narrow range of materials and roofing approaches. The area as a whole is brought together through the simple and repeated approach to boundaries in brick with greenery behind, and new development could use the variation and similarity at the appropriate scale to add interest without breaking from the essential character of Misson.

Dame Lane

Dame Lane connects two locally important streets and is an example of a back lane, many of which can be found in Misson and not all of which have been selected for individual study. It is denser, tighter knit and has a high level of enclosure to its edge whilst also providing a more regular approach to its townscape through buildings sitting in a regular arrangement on their plots, generally facing the street.

Plot, building and street dimensions typically found in the area.
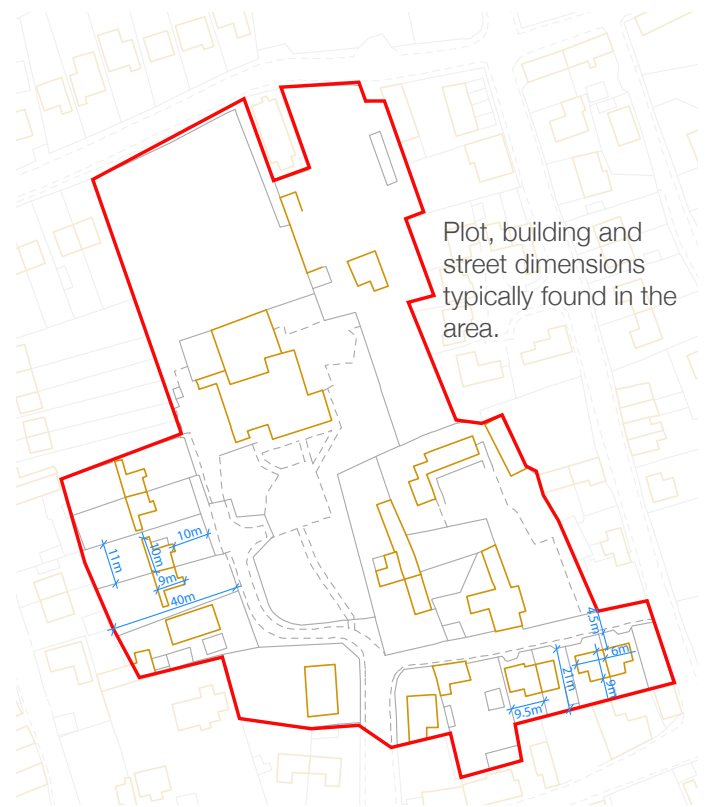
Materials

Red-brown bricks, with a mix of bonds.

Some tile hanging, with white weather boarding.

Some cream render, used sparingly. Brick course detailing to eaves.

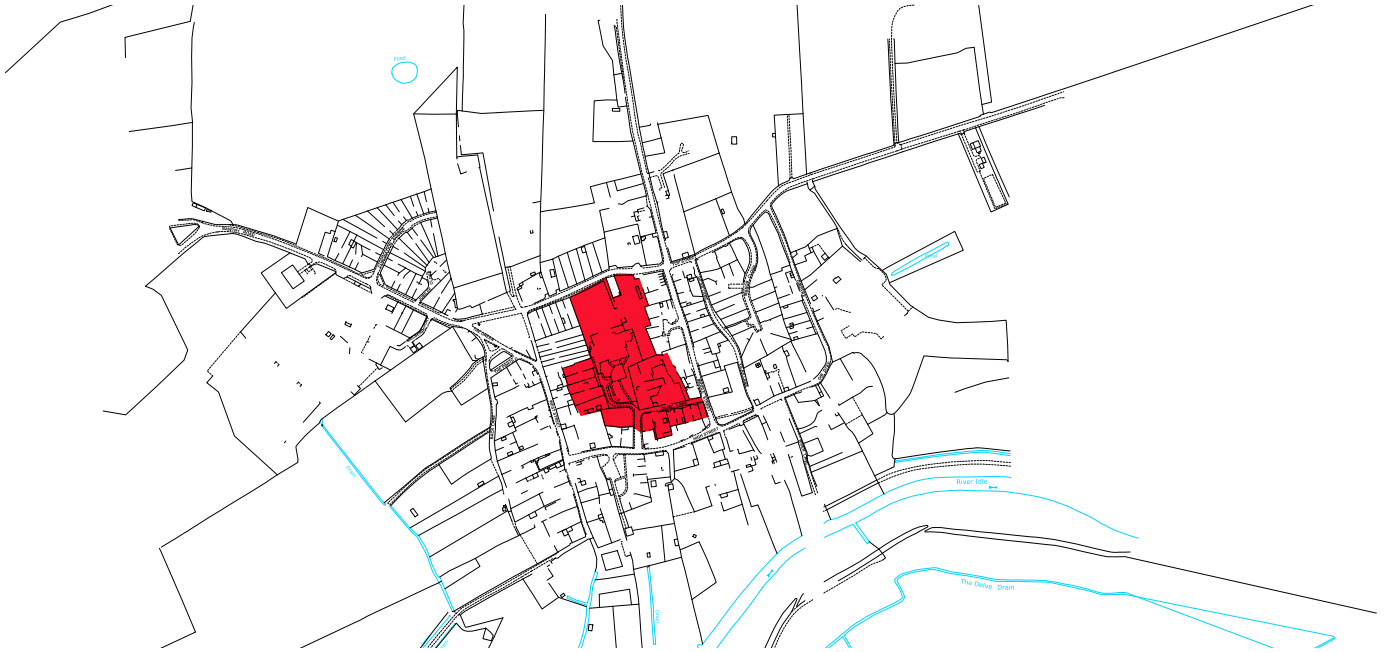Red and brown pantile roofs.

Details

Low walls in red brick with English garden bond.

Hedges to back of front boundaries, adding greener to street.

Garden trees appear as street trees, adding to the feeling of enclosure and greenery in the public street.

| Street role and form.	Local grid street, connecting to other local routes rather then to places beyond the village. The street acts as a shared surface at time, with only narrow footpaths.
Plot range	For the most part narrow, with plots falling in the 9-12m width range, although some special buildings like the pub sit on much larger plots.
Building to plot ratio	High, typically 70-90%, although some buildings on larger plots fall to around 40%.
Building set back	Buildings tend to be clustered in groups with similar set backs, which range from 10m for some, 4.5m for others but with larger buildings sat hard against the edge of the footpath.
Roof scape	Mainly open gables with the ridge running parallel to the street. Hipped roofs to some dwellings. Roofing materials are red pantiles and most roofs have chimneys.
Parking	Mainly on-plot to the side of dwellings, some garages. On street parking also features.
Landscape and boundaries	Front boundaries consist of low walls in brick with coping course to top. Some hedging below. Trees in front gardens add greenery to the street.
Scale and massing	Two storey, with single storey porch structures to some properties. Eaves height approximately 6m, 9m to ridge.

Lessons

Dame Lane is a reasonably regular street, with more repetition than many of the streets in Mission. This repetition may be useful for new development in some instances, enabling individual streets to have elements that work together to reinforce character. Many of the buildings are not reflective of the best Misson has to offer, so new development should combine some of the design approaches here with more place-specific design at the detailed level. The consistent boundary treatment works well and should act as a useful precedent.

Middle Street

Middle Street is a local grid road that connects between the core of the village and the streets that integrate to settlements beyond. It provides a visual link from the main streets that pass through the village to the church at the junction of locally important streets. It has a dense feel, with more regularity in how the buildings and street sit in relation to one another. There is a narrow range of materials, and high walls are commonplace as boundary treatments.
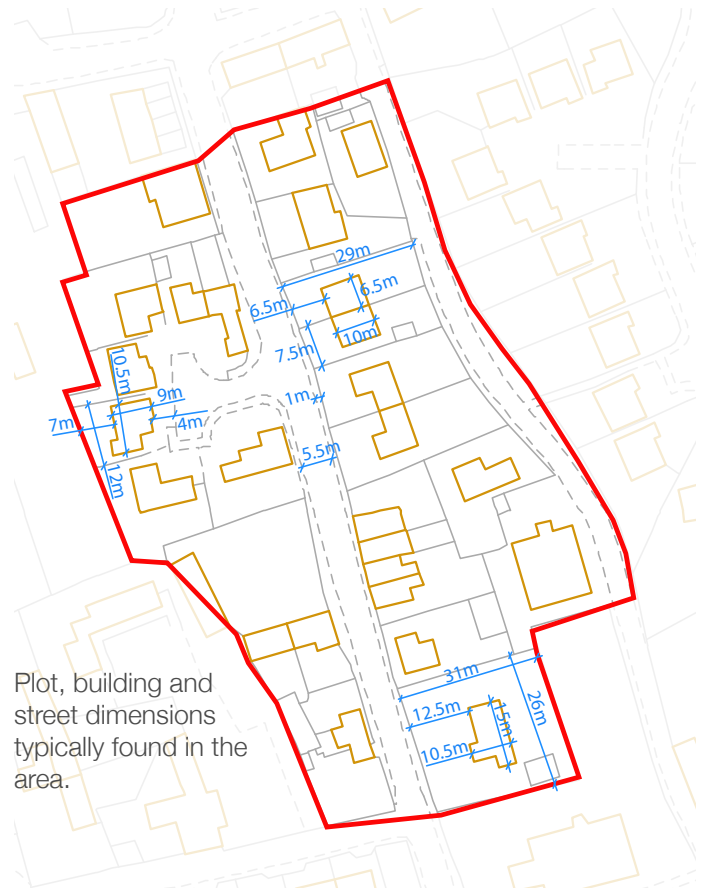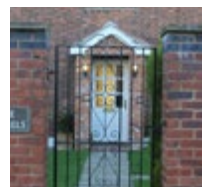

Plot, building and street dimensions typically found in the area.
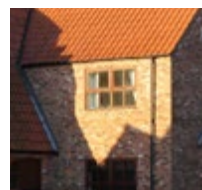
Materials

Multi-grain bricks, mainly reds and browns.

Red clay pantiles for many roofs.

Some render for return faces.

Details

High walls with brick course detailing, some blue bricks present.

Coloured window frames and hip and valley roofs.
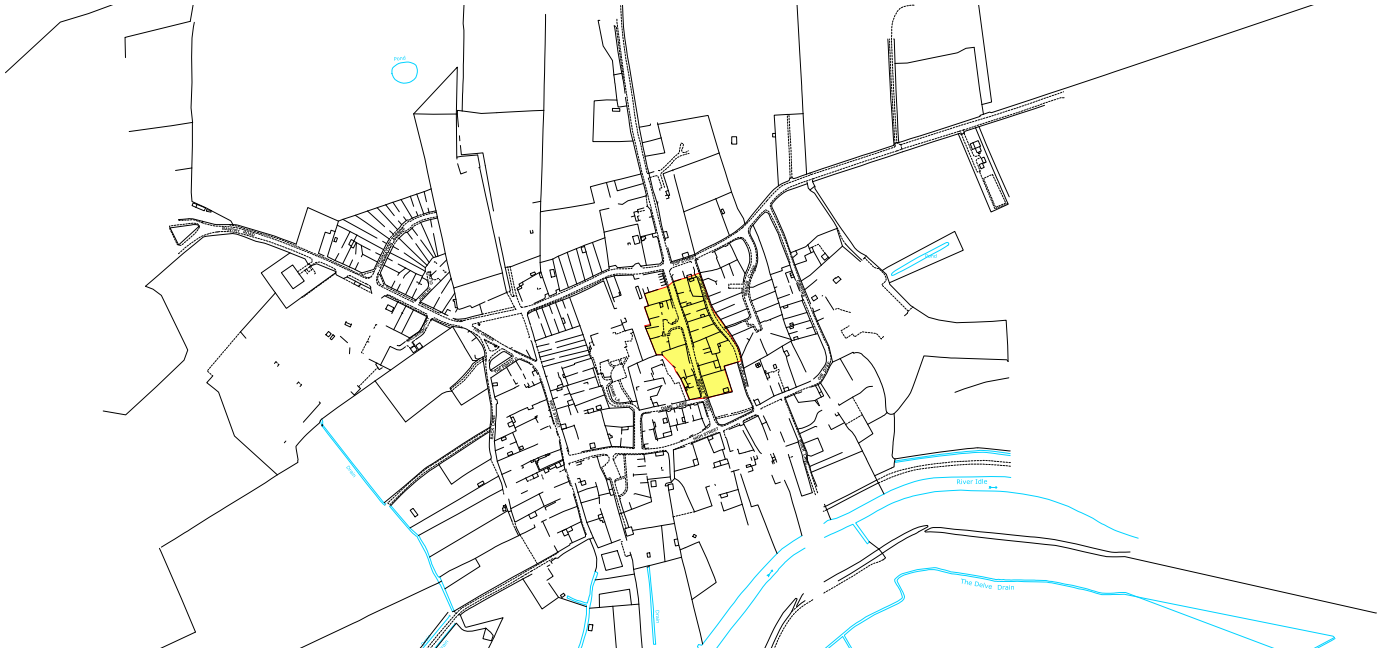

On street, although not well integrated into the street.

| Street role and form.	Local grid street, connecting to other local routes rather then to places beyond the village. Narrow footpaths to both sides. Sight lines of approximately 200+m, with no deflections in alignment.
Plot range	Reasonably narrow, with clusters of buildings with similar plots. Plots are typically 8-25m in width, clustering around the 10-15m mark.
Building to plot ratio	High, typically 70-80%, although some houses occupy 100% of the plot width.
Building set back	Buildings tend to be in clusters, so that groups have similar set back. This generally ranges from 6-9m, although some buildings sit hard against the edge of the footpath.
Roof scape	Mainly open gables with the ridge running parallel to the street. Front projecting gables perpendicular to street some dwellings. Roofing materials are either red of brown clay pantiles.
Parking	Mainly on-street to the front of dwellings. Some on-plot to the side of dwellings. Garages are present but infrequent.
Landscape and boundaries	Front boundaries consist high walls in brick, multi-grain and with a variety of bonds. Some hedging behind, but not commonplace.
Scale and massing	Two storey, with garage structures to some of the properties. Eaves height approximately 6m, 9m to ridge.

Lessons

Middle Street is one of the more urban and dense streets in Misson, with a hard boundary edge to the street space, very regular street alignment and smaller plots. The buildings span a fair amount of their plots, making the space feel enclosed and focusing views along the street length rather than offering glimpses through to other parts of the village. This street frames views of the church well, and new development can use this technique to make sure they incorporate the wider setting into the design. The simple pallet of materials and consistent boundary help this space work as one.

Manor Close

Manor Close is an infill housing area that sits adjacent to Top Road. It is a mix of two storey housing and bungalows arranged around a long, sinuous cul-de-sac, atypical of wider Misson. It uses a red wire-cut brick, with plain detailing to the buildings and less formal boundaries than found in other parts of the village.
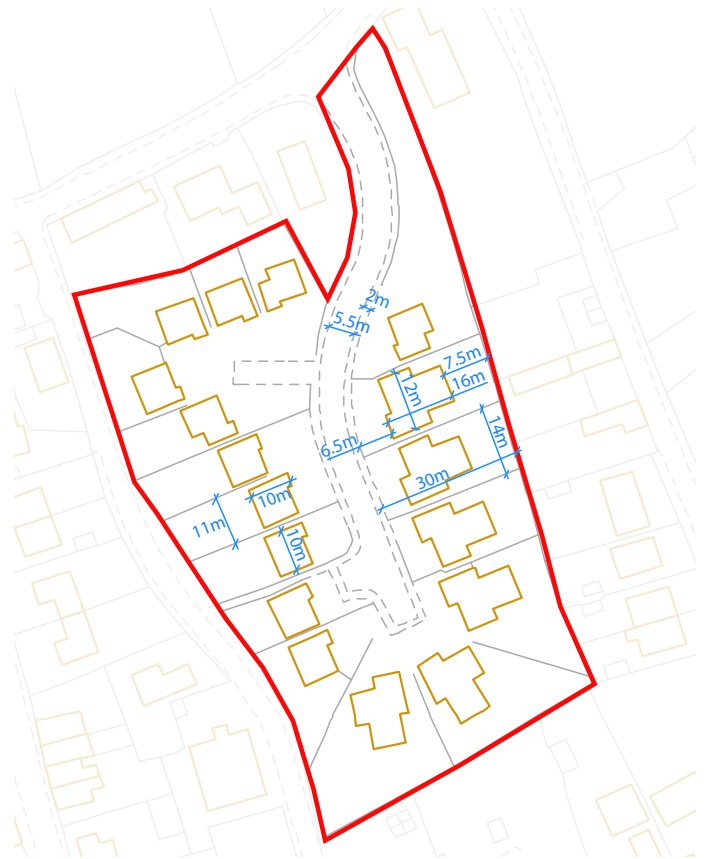
Materials

Wire-cut red bricks with white uPVC and weatherboarding.

Brown pantiles with white soffits.

Details

Some hedges in gardens but inconsistent. A recent trend to replace hedges with post and rail fencing is eroding the character of the street.
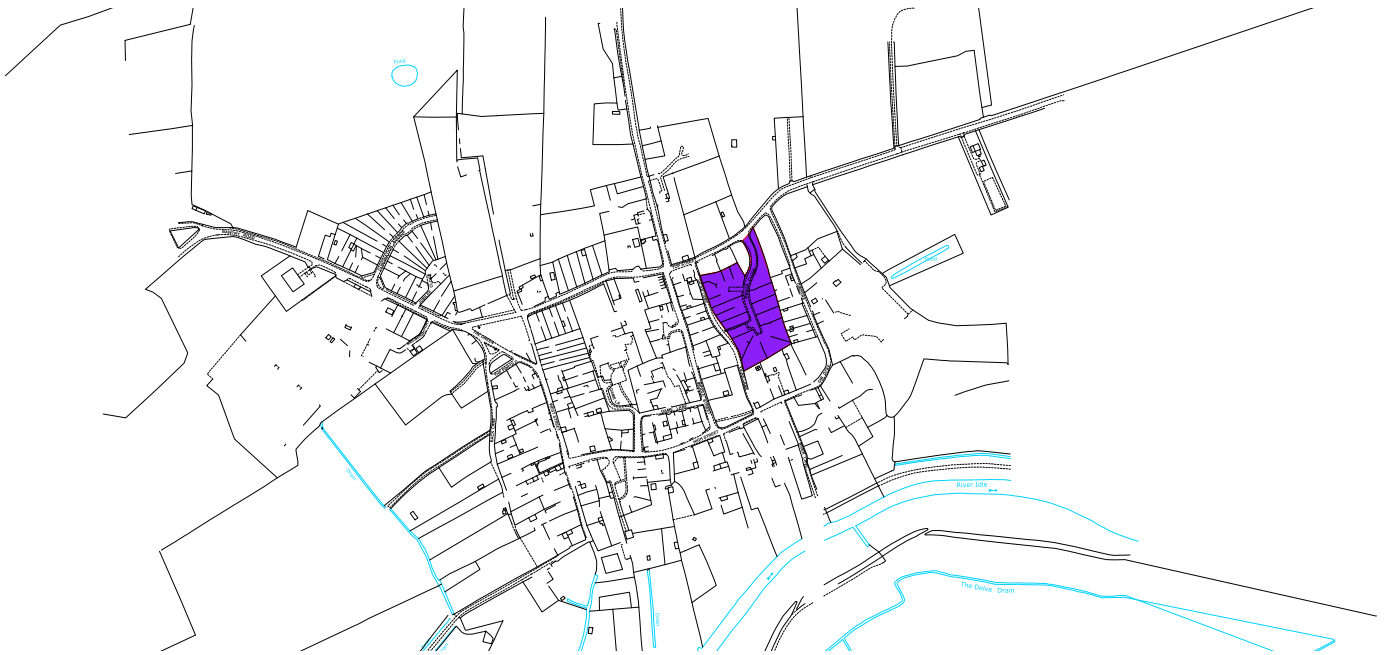
Chimneys to some properties. Integral garages also feature.

Small clusters of homes with open frontages around a communal access.

Some front-facing gables on bungalows.

| Street role and form.	Local access only, with no connecting role for wider movement. Standard dimensions, with pavement to either side. Sight lines of approximately 50m, with sinuous angular deflections.
Plot range	Very narrow, typically similar in width although varied in length to allow for the shape of the site. 10-12m plot width is used, with lengths from 30-40m.
Building to plot ratio	High, typically 80-90%, with buildings sitting centrally in their plot and alleyways running along the sides.
Building set back	Relatively uniform at around 6m, but with a few properties arranged around a shared access spur giving the impression of greater set back.
Roof scape	Mainly open gables with the ridge running parallel to the street. Front projecting gables perpendicular to street some dwellings. Roofing materials are brown clay pantiles. Some chimneys.
Parking	Mainly on-plot to the front of dwellings, with some integral garages. Limited scope for on-street parking.
Landscape and boundaries	A mix of open frontages, low shrub planting, plus some mono-culture hedge. Post and rail fencing for some plots, although not encouraged.
Scale and massing	A mix of single and two storey, with the shallow roofs of the single storey dwellings making them appear lower still.

Lessons

There is a uniformity of approach to Manor Close which gives it its own character, but this character is not reflective of wider Misson. The simple arrangement of buildings to the street works well, as does adequate on-plot parking, but the lack of a consistent high-quality boundary treatment and low levels of detailing at the building level makes this part of the village less in keeping with the wider place. The character of the street is under threat from people removing the hedge boundaries; effort should be made to ensure the hedges are kept. New development should seek to inject more local materials, a more varied streetscape and stronger hedge boundaries to ensure that new development does not erode the overall character of Misson.

Top Street / Top Road

Top Street and Top Road are the main integrators for village, providing connectivity to the west and east and passing through important junctions that enable access to the core of the village. Top Road manages the transition from the rural edge of the settlement through to the more urbanised core, and Top Street does the same on the east.
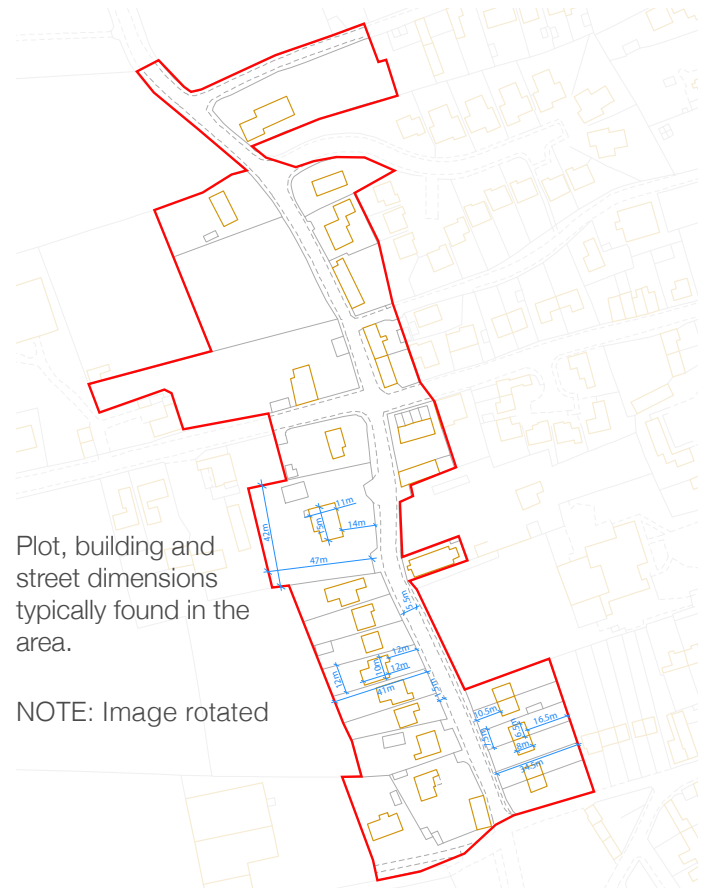
Plot, building and street dimensions typically found in the area.

NOTE: Image rotated

Materials

Roofs are clay pantiles in red or brown.

Some render in white, with a rustic finish.

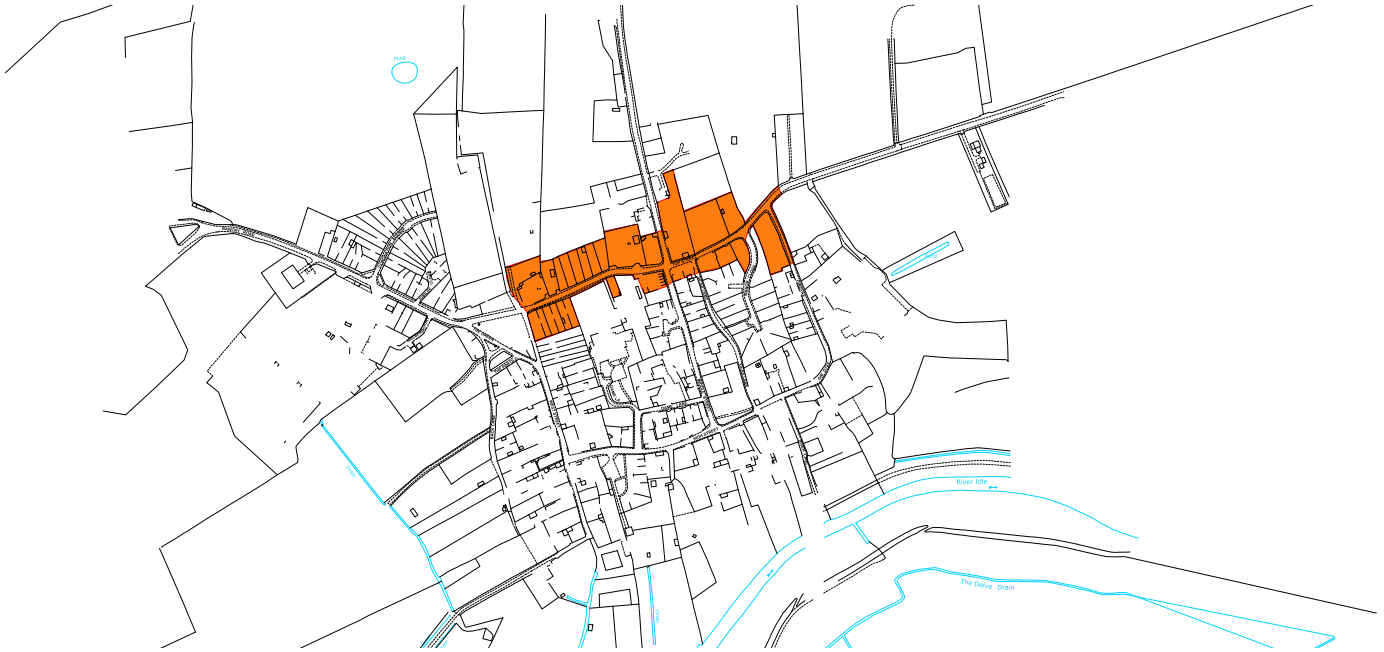Multi-grain bricks with Flemish bond.

Details

Medium and high walls form most of the boundaries, usually in red brick.

Some plots have black railings atop a plinth wall.

Feature buildings help to announce this street as important, denoting its status in the movement hierarchy.

Street role and form.	Global integrator, offering access to settlements beyond the village. Narrower and more minor that the other integrators in the village. Foot path to both sides, sight lines long at approximately 160m.
Plot range	Reasonably wide, although plots tend to be grouped. Plots are tighter on Top Street (around 12m) and much wider towards the cross roads (20-45m).
Building to plot ratio	Varied, although typically 80-90%. Some larger houses occupy around 50% of the plot width, but this is uncommon.
Building set back	Relatively uniform at around 10-12m, but with a few properties sitting back up to 20m, although infrequently.
Roof scape	Mainly open gables with the ridge running parallel to the street. Front projecting gables perpendicular to street some dwellings, infrequent hip and valley roofing in some places. Roofing materials are a mix of brown and red pantiles, with some slate.
Parking	Mainly on-plot to the front of dwellings, with some side-of-plot parking also present. Some garages set back to the rear of plot.
Landscape and boundaries	Front boundaries consist of multi-grain brick walls of varying heights, although typically around 1.2m. Some hedges. Garden trees add greenery to the street edge.
Scale and massing	A mix of single, two and three storey buildings, with taller buildings tending to be clustered around junctions.

Lessons

Top Street and Top Road are varied streets, with grand buildings on large plot interspersed with smaller buildings on more regular plots. A relatively consistent boundary treatment unifies the street and allows for variation at the building level without it feeling incongruous. New development looking to add important streets should replicate the direct accessed on-plot parking, the narrow range of materials and the walls to boundary edges. Space should be made for trees in front gardens, as these help to green the street.

Station Road

Station Road is the main integrating street for places to the north of Misson. It provides a key part of the central cross-roads for all the major streets in the village, and manages the transition from the rural edge of the village to the more urban core.

Plot, building and street dimensions typically found in the area.

Materials

Red bricks with a multi-grain mix, Flemish bond.
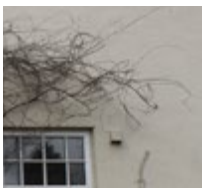
Rustic render in whites and creams.

Brown and red pantiles for roofs.
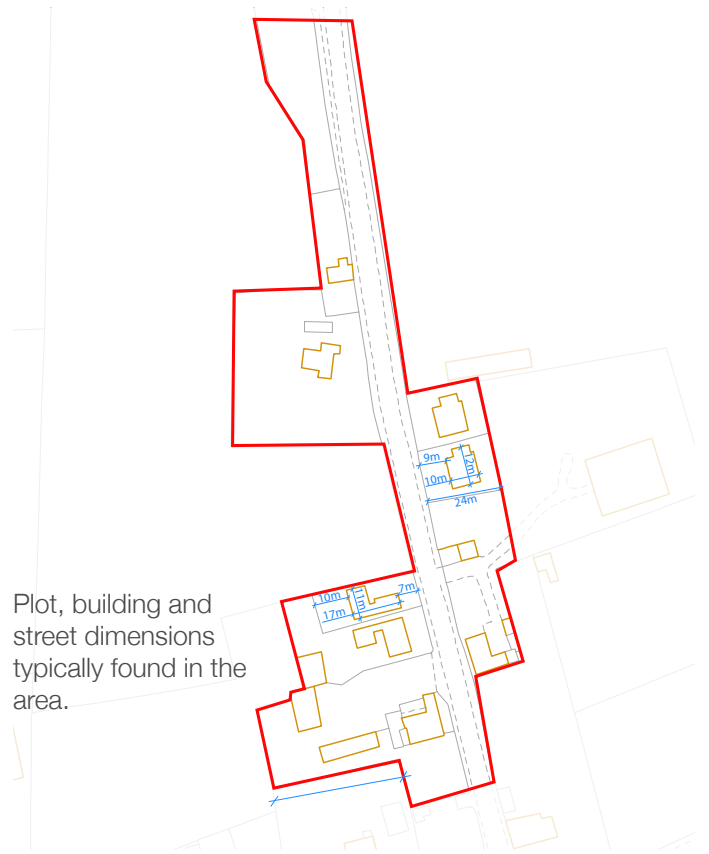
Details

Medium to high walls in brick, with hedging behind.

Rural transition uses field hedging to edge of street, verges and no footpath.

Courtyard development to add infill and dwelling mix.

Some buildings sit to the edge of the footpath forming the boundary with their flanks.

Street role and form.	Global integrator, offering access to settlements beyond the village. Narrower and more minor that the other integrators in the village. Foot path to both sides, sight lines long at approximately 280m.
Plot range	Very varied, from regular plots that sit perpendicular to the street through to very large plots that present their long edge to the carriageway. Most plots sit within the 18-30m range.
Building to plot ratio	Extremely varied, but generally low with around 20% typical. Some properties range up to 80%, although these tend to be clustered.
Building set back	Relatively uniform at around 6m, but with a few properties on the inside of corners setting back at 12m.
Roof scape	Very mixed, but with more hip and valley roofs than in other parts of the village. Some hipped roofs, others with gables running parallel to the building face (although not always parallel to the street).
Parking	Mainly on-plot to the side of dwellings. Some garages. Incidental on-street parking evident.
Landscape and boundaries	Mainly rural hedging in a variety of species, with the look and feel of field boundaries. Some high walls in brick. Large trees in back gardens are visible.
Scale and massing	1.5 to two storey, with single storey outbuildings to many properties. Varied eaves and ridge height, with some larger builds towards the village.

Lessons

Station Road offers a gradual transition from the rural edge of the village through to the main streets, with large hedges and a fragments look and feel making this street act in part as a county lane and in part as a main route within town. New development looking to provide important new routes but that wish to maintain a more rural feel could emulate the landscaping, good sight lines and large plots. The simple range of materials helps to unify the street, whilst the varied roofscape adds character and interest.

Part 3:
Appendices

Appendix 1: Manual for Streets

Manual for Streets (MfS) replaces Design Bulletin 32, first published in 1977, and its companion guide Places, Streets and Movement.

It puts well-designed residential streets at the heart of sustainable communities. For too long the focus has been on the movement function of residential streets. The result has often been places that are dominated by motor vehicles to the extent that they fail to make a positive contribution to the quality of life.

MfS demonstrates the benefits that flow from good design and assigns a higher priority to pedestrians and cyclists, setting out an approach
to residential streets that recognises their role in creating places that work for all members of the community. MfS refocuses on the place function of residential streets, giving clear guidance on how to achieve well-designed streets and spaces that serve the community in a range of ways.

MfS updates the link between planning policy and residential street design. It challenges some established working practices and standards that are failing to produce good-quality outcomes, and asks professionals

to think differently about their role in creating successful neighbourhoods.

It places particular emphasis on the importance of collaborative working and coordinated decision-making, as well as on the value of strong leadership and a clear vision of design quality at the local level.

Research carried out in the preparation of Manual for Streets indicated that many of the criteria routinely applied in street design are
based on questionable or outdated practice.

For example, it showed that, when long forward visibility is provided and generous carriageway width is specified, driving speeds tend to
increase. This demonstrates that driver behaviour is not fixed; rather, it can be influenced by the environment.

MfS addresses these points, recommending revised key geometric design criteria to allow streets to be designed as places in their own right while while still ensuring that road safety is maintained.

46

Manual for
Streets, TFL
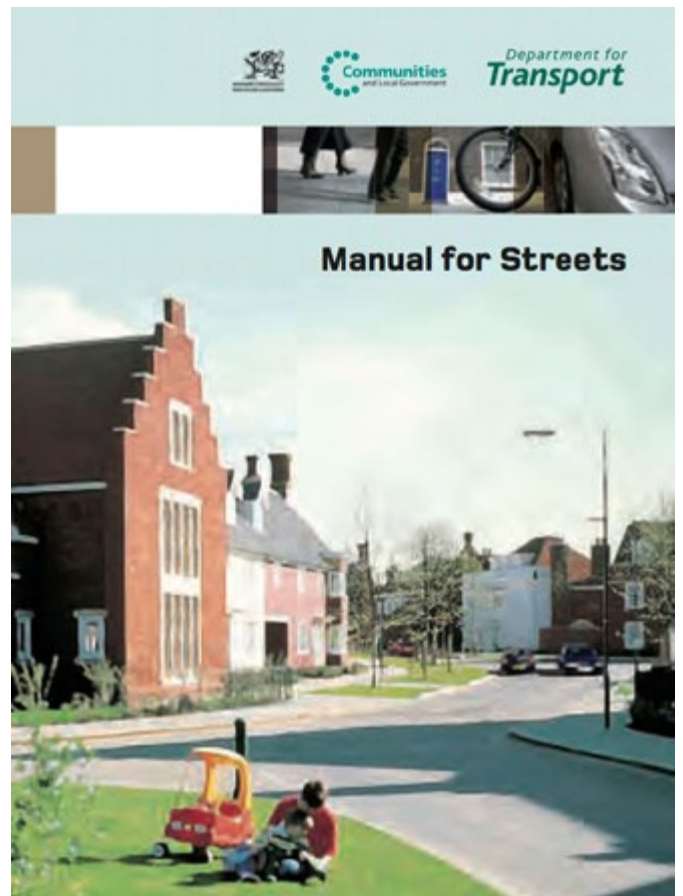
Appendix 2: Building for Life 12

Building for Life 12 is the industry standard, endorsed by government for well-designed homes and neighbourhoods that local communities, local authorities and developers are encouraged to use to help stimulate conversations about creating good places to live.

The 12 questions reflect our vision of what new housing developments should be: attractive, functional and sustainable places. Redesigned in 2012, BfL12 is based on the National Planning Policy Framework and the government's commitment to not only build more homes, but better homes - whilst also encouraging local communities to participate in the place making process.

The questions are designed to help structure discussions between local communities, local planning authorities, developers and other stakeholders.

BfL12 is also designed to help local planning authorities assess the quality of proposed and completed developments; it can be used for site-specific briefs and can also help to structure design codes and local design policies.

BfL12 comprises of 12 easy to understand questions that are designed to be used as a way of structuring discussions about a proposed development. There are four questions in each of the three chapters:

• Integrating into the neighbourhood
• Creating a place
• Street and home

Based on a simple 'traffic light' system (red, amber and green) we recommend that proposed new developments aim to:
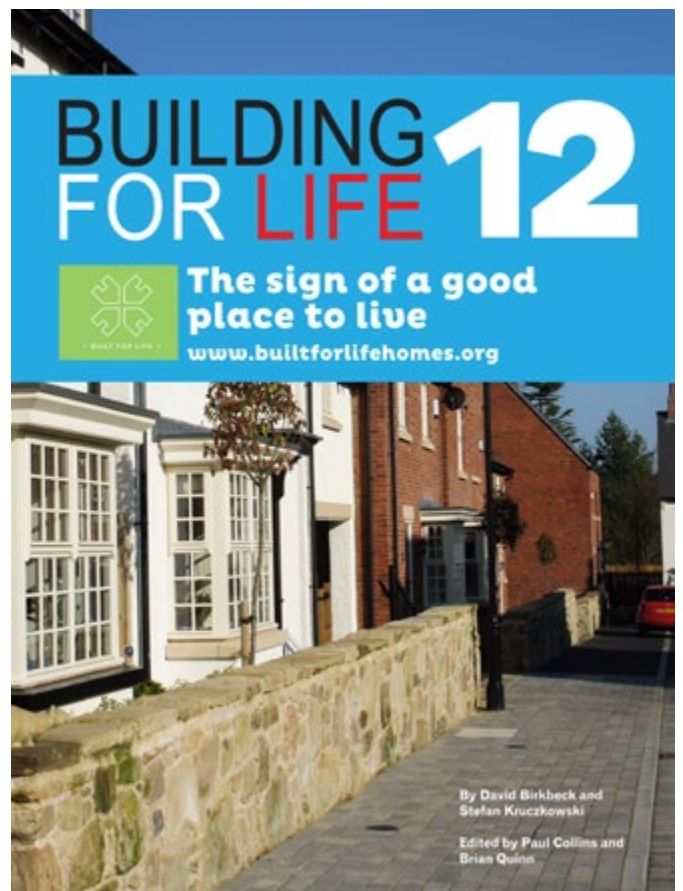
• Secure as many 'greens as possible,
• Minimise the number of 'ambers' and;
• Avoid 'reds'.

The more 'greens' that are achieved, the better a development will be.

A red light gives warning that a particular aspect of a proposed development needs to be reconsidered.

Building for
Life 12, the BfL
Partnership

Appendix 3: Glossary of terms

*Shortened extracts from By Design (ODPM/CABE, 2000)
and The Dictionary of Urbanism (Streetwise Press, 2003)*

accessibility The ease with which a building, place or facility can be reached by people and/or goods and services. Accessibility can be shown on a plan or described in terms of pedestrian and vehicle movements, walking distance from public transport, travel time or population distribution.

adaptability The capacity of a building or space to respond to changing social, technological, economic and market conditions.

amenity Something that contributes to an area's environmental, social, economic or cultural needs. The term's meaning is a matter for the exercise of planners' discretion, rather than being defined in law.

appearance Combination of the aspects of a place or building that determine the visual impression it makes.

area appraisal An assessment of an area's land uses, built and natural environment, and social and physical characteristics.

authenticity The quality of a place where things are what they seem: where buildings that look old are old, and where the social and cultural values that the place seems to reflect did actually shape it.

background building A building that is not a distinctive landmark.

backland development The development of sites at the back of existing development, such as back gardens. barrier An obstacle to movement.

best value The process through which local authorities work for continuous improvement in the services they provide. Local authorities are required to challenge why a particular service is needed; compare performance across a range of indicators; consult on the setting of new performance targets; and show that services have been procured through a competitive process. Councils are subject to independent best value audits by the Best Value Inspectorate, an offshoot of the Audit Commission. block The area bounded by a set of streets and undivided by any other significant streets.

block The space in between the streets, usually used for development but can also be used for parkland and open space. The shape can be regular (square) or rectilinear (longer and shorter sides).

brief This guide refers to site-specific briefs as development briefs. Site-specific briefs are also called a variety of other names, including design briefs, planning briefs and development frameworks.

building element A feature (such as a door, window or cornice) that contributes to the overall design of a building.

building line The line formed by the frontages of buildings along a street. The building line can be shown on a plan or section.

building shoulder height The top of a building's main facade.

built environment The entire ensemble of buildings, neighbourhoods and cities with their infrastructure.
built form Buildings and structures.

bulk The combined effect of the arrangement, volume and shape of a building or group of buildings. Also called massing.

character appraisal Techniques (particularly as developed by English Heritage) for assessing the qualities of conservation areas.

character area An area with a distinct character, identified as such so that it can be protected or enhanced by planning policy. The degree of protection is less strong than in a conservation area.

character assessment An area appraisal emphasising historical and cultural associations.

conservation area character appraisal A published document defining the special architectural or historic interest that warranted the area being designated.
conservation area One designated by a local authority under the Town and Country Planning (Listed Buildings and Conservation Areas) Act 1990 as possessing special architectural or historical interest. The council will seek to preserve or enhance the character and appearance of such areas.

context (or site and area) appraisal A detailed analysis of the features of a site or area (including land uses, built and natural environment, and social and physical characteristics) which serves as the basis for an urban design framework, development brief, design guide, or other policy or guidance.
context The setting of a site or area.

countryside design summary A descriptive analysis explaining the essential design relationship between the landscape, settlement patterns and buildings. From this analysis the document draws principles that can be applied to development in the area and sets out the implications of the choices open to designers. As supplementary planning guidance prepared by a local authority, the summary can encourage a more regionally and locally based approach to design and planning. It can also provide the context for individual communities to prepare village design statements.

defensible space Public and semi-public space that is 'defensible' in the sense that it is surveyed, demarcated or maintained by somebody. Derived form Oscar Newman's 1973 study of the same name, and an important concept in securing public safety in urban areas, defensible space is also dependent upon the existence of escape routes and the level of anonymity which can be anticipated by the users of space.

density The mass or floorspace of a building or buildings in relation to an area of land. Density can be expressed in terms of plot ratio (for commercial development); homes or habitable rooms per hectare (for residential development); site coverage plus the number of floors or a maximum building height; space standards; or a combination of these.

design code A document (usually with detailed drawings or diagrams) setting out with some precision the design and planning principles that will apply to development in a particular place.

design guidance A generic term for documents providing guidance on how development can be carried out in accordance with the planning and design policies of a local authority or other organisation.

design guide Design guidance on a specific topic such as shopfronts or house extensions, or relating to all kinds of development in a specific area.

design policy Relates to the form and appearance of development, rather than the land use.

design principle An expression of one of the basic design ideas at the heart of an urban design framework, design guide, development brief or design code. Each such planning tool should have its own set of design principles.

design statement A developer can make a pre-application design statement to explain the design principles on which a development proposal in progress is based. It enables the local authority to give an initial response to the main issues raised by the proposal. An applicant for planning permission can submit a planning application design statement with the application, setting out the design principles adopted in relation to the site and its wider context. Government advice (Planning Policy Guidance Note 1) encourages an applicant for planning permission to submit such a written statement to the local authority.

design-led development (or regeneration) Development whose form is largely shaped by strong design ideas.

desire line An imaginary line linking facilities or places which people would find it convenient to travel between easily.

development appraisal A structured assessment of the characteristics of a site and an explanation of how they have been taken into account in drawing up development principles.

development brief A document providing guidance on how a specific site of significant size or sensitivity should be developed in line with the relevant planning and design policies. It will usually contain some indicative, but flexible, vision of future development form. A development brief usually covers a site most of which is likely to be developed in the near future. The terms 'planning brief' and 'design brief' are also sometimes used. These came into use at a time when government policy was that planning and design should be kept separate in design guidance. The term 'development brief' avoids that unworkable distinction.

development control The process through which a local authority determines whether (and with what conditions) a proposal for development should be granted planning permission.

development plan Prepared by a local authority to describe the intended use of land in an area and provide a basis for considering planning applications. Every area is covered either by a unitary development plan or by a development plan comprising more than one document (a structure plan and a local plan, and sometimes also other plans relating to minerals and waste). The development plan sets out the policies and proposals against which planning applications will be assessed. Its context is set by national and regional planning policy guidance.

development Statutorily defined under the Town and Country Planning Act 1990 as 'the carrying out of building, engineering, mining or other operation in, on, over or under land, or the making of any material change in the use of any building or other land'. Most forms of development require planning permission.

eyes on the street People whose presence in adjacent buildings or on the street make it feel safer.

facade The principal face of a building.

fenestration The arrangement of windows on a facade. figure/ground (or figure and ground diagram) A plan showing the relationship between built form and publicly accessible space (including streets and the interiors of public buildings such as churches) by presenting the former in black and the latter as a white background, or the other way round.

fine grain The quality of an area's layout of building blocks and plots having small and frequent subdivisions.

form The layout (structure and urban grain), density, scale (height and massing), appearance (materials and details) and landscape of development.

grid (street pattern) A street system in which streets connect at both ends with other streets to form a grid-like pattern. Grids can be regular or deformed; regular grids have junctions that meet at crossroads, whereas deformed grids have their junctions offset from one another.

in-curtilage parking Parking within a building's site boundary, rather than on a public street or space.

landmark A building or structure that stands out from the background buildings.

landscape The appearance of land, including its shape, form, colours and elements, the way these (including those of streets) components combine in a way that is distinctive to particular localities, the way they are perceived, and an area's cultural and historical associations.

layout The way buildings, routes and open spaces are placed in relation to each other.

legibility The degree to which a place can be easily understood by its users and the clarity of the image it presents to the wider world.

live edge Provided by a building or other feature whose use is directly accessible from the street or space which it faces; the opposite effect to a blank wall.

local distinctiveness The positive features of a place and its communities which contribute to its special character and sense of place.
massing The combined effect of the arrangement, volume and shape of a building or group of buildings. This is also called bulk.

mixed uses A mix of complementary uses within a building, on a site or within a particular area. 'Horizontal' mixed uses are side by side, usually in different buildings. 'Vertical' mixed uses are on different floors of the same building.

movement People and vehicles going to and passing through buildings, places and spaces.

natural surveillance (or supervision) The discouragement to wrong-doing by the presence of passers-by or the ability of people to see out of windows. Also known as passive surveillance (or supervision).

node A place where activity and routes are concentrated. performance criterion/criteria A means of assessing the extent to which a development achieves a particular

urban forward ltd